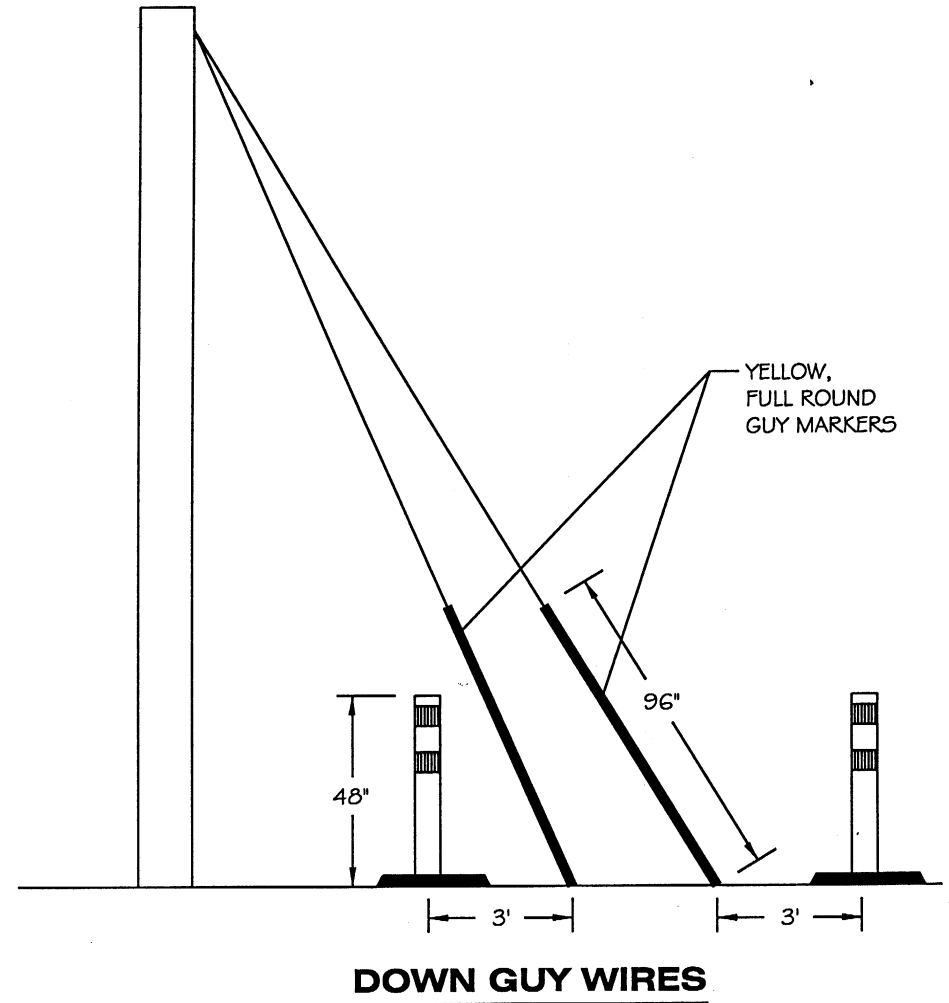
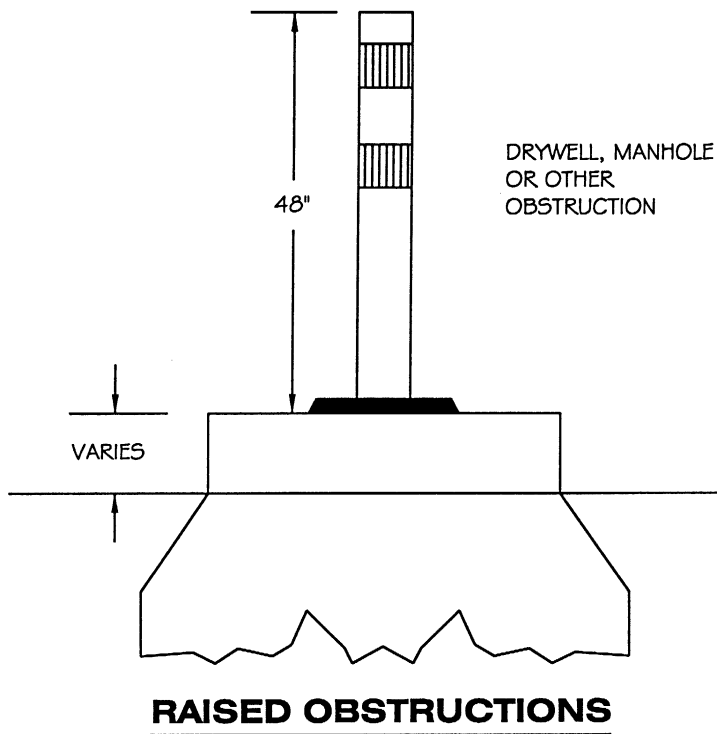


GENERAL INFORMATION

C-100 TO C-111



NOTES:

1. HAZARD MARKERS SHALL BE INSTALLED WHEN AN OBSTRUCTION MAY POSE A HAZARD TO MOTORIZED VEHICLES, MOWING EQUIPMENT, PEDESTRIANS, MAINTENANCE OPERATIONS OR AS DIRECTED BY THE ENGINEER.
2. HAZARD MARKERS SHALL MEET THOSE SPECIFICATIONS AS SHOWN IN C.O.C. STANDARD DETAIL NO. C-101 OR NO. C-102.
3. C.O.C. DETAIL NO. C-102 MAY BE USED AROUND RAISED OBSTRUCTIONS AS AN ALTERNATE; HOWEVER, THREE (3) MARKERS MUST BE PLACED AT 120° INTERVALS AROUND THE OBSTRUCTION.

C-100
REPLACES
62



CITY OF
CHANDLER
STANDARD
DETAIL

HAZARD MARKER PLACEMENT

APPROVED:

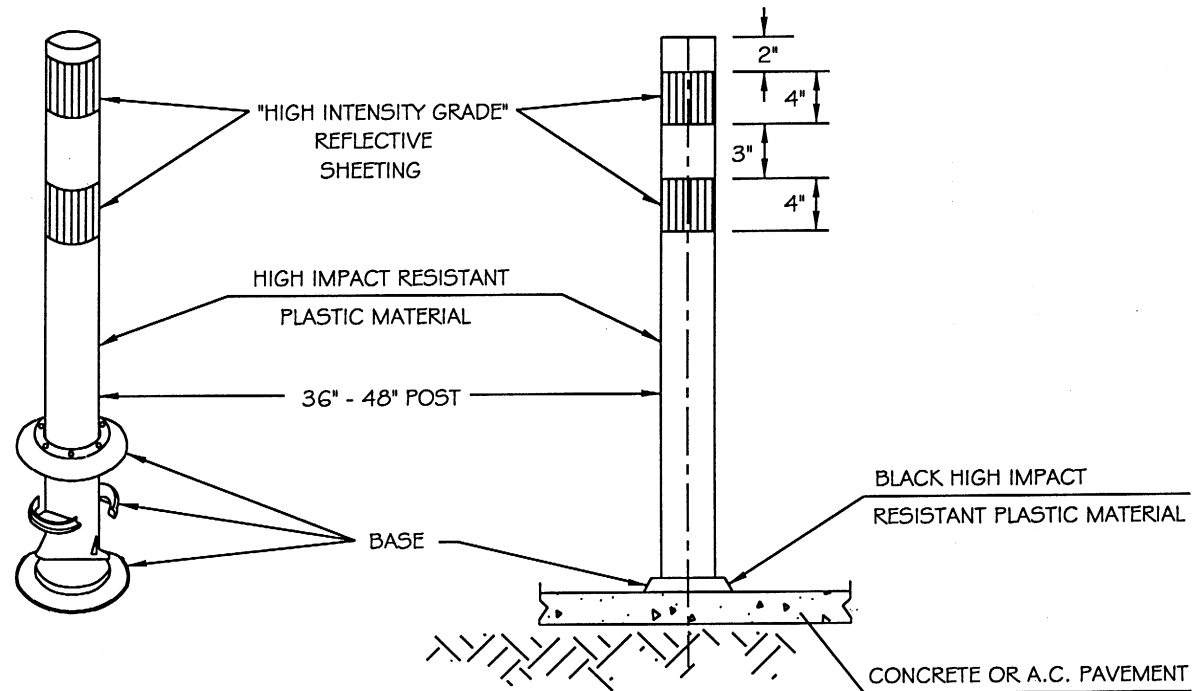
DATE: 11-19-99

CITY ENGINEER

DETAIL NO.

C-100

NTS



NOTES:

1. CONTRACTOR SHALL CLEAN ROADWAY SURFACE PRIOR TO PLACEMENT OF FLEXIBLE TUBULAR MARKER.
2. FLEXIBLE TUBULAR MARKERS SHALL BE INSTALLED WITH AN ADHESIVE AS PER TUBULAR MARKER MANUFACTURER'S SPECIFICATIONS.
3. YELLOW TUBULAR MARKERS SHALL HAVE A YELLOW POST AND YELLOW "HIGH INTENSITY GRADE" REFLECTIVE SHEETING. ORANGE TUBULAR MARKERS SHALL HAVE AN ORANGE POST AND WHITE HIGH INTENSITY REFLECTIVE SHEETING.
4. POST SHALL BE FLEXIBLE, HIGH IMPACT RESISTANT PLASTIC MATERIAL.
5. BASE SHALL BE CEMENTED TO SURFACE WITH EPOXY MATERIAL AS RECOMMENDED BY THE MANUFACTURER.

C-101
REPLACES
63

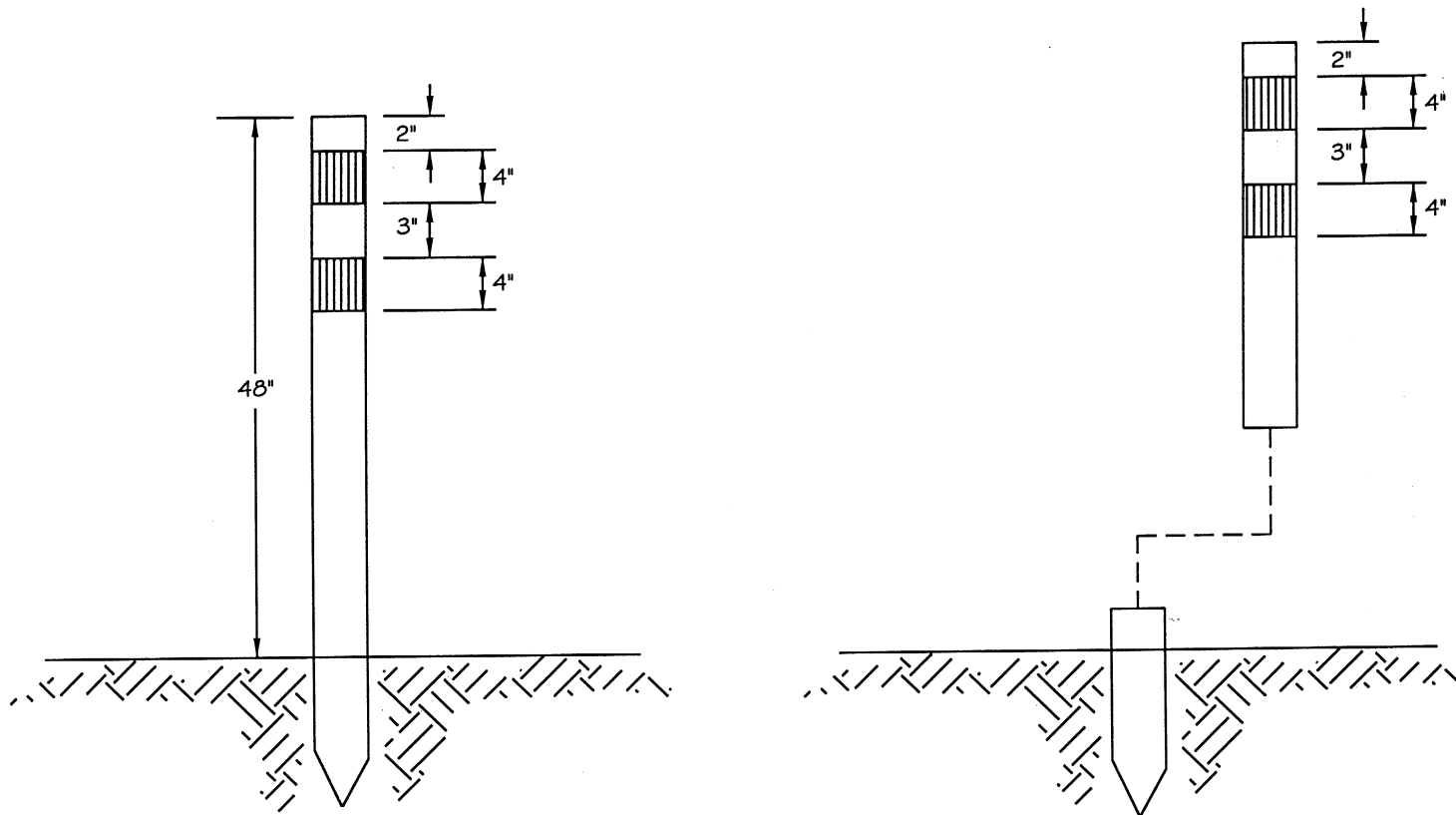


CITY OF
CHANDLER
STANDARD
DETAIL

HAZARD MARKER

APPROVED: *Bryan D. Peterson*
CITY ENGINEER
DATE: *11-19-99*

DETAIL NO.
C-101
NTS



NOTES:

1. YELLOW TUBULAR MARKERS SHALL HAVE A YELLOW POST AND YELLOW "HIGH INTENSITY GRADE" REFLECTIVE SHEETING. ORANGE TUBULAR MARKERS SHALL HAVE AN ORANGE POST AND WHITE HIGH INTENSITY REFLECTIVE SHEETING.
2. POST SHALL BE FLEXIBLE, HIGH IMPACT RESISTANT PLASTIC MATERIAL.

C-102
REPLACES
64



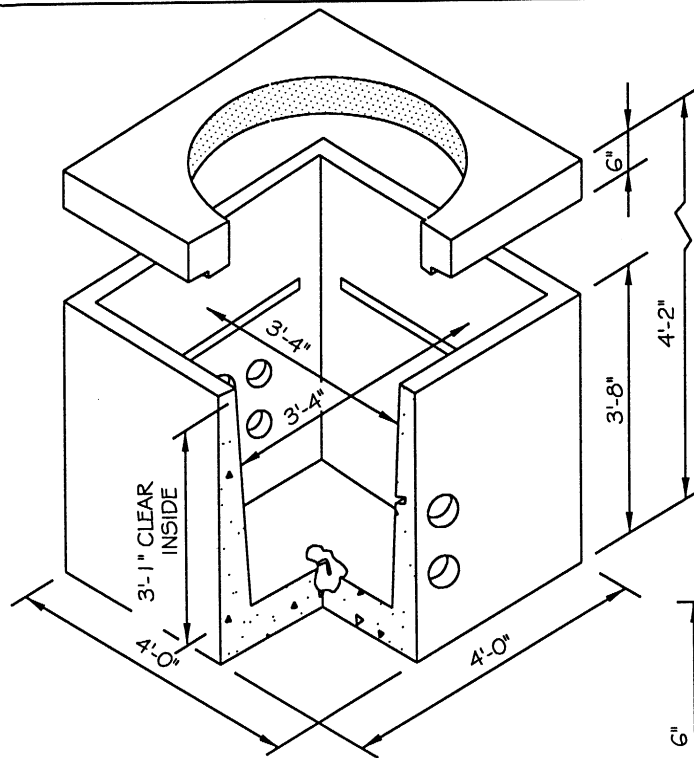
CITY OF
CHANDLER
STANDARD
DETAIL

**HAZARD MARKER
SOIL ANCHOR**

APPROVED: *Bryan D. Dalton*
CITY ENGINEER
DATE: *11-19-99*

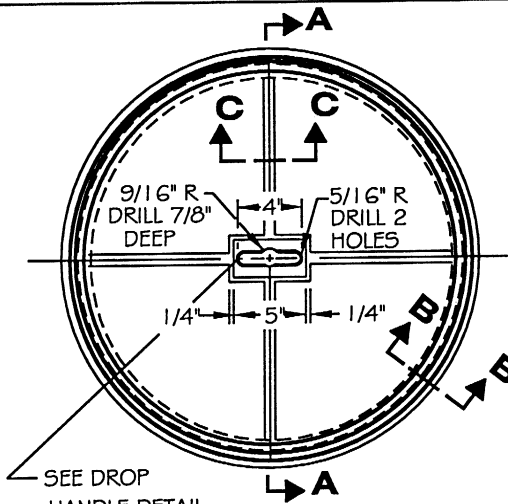
DETAIL NO.

C-102
NTS

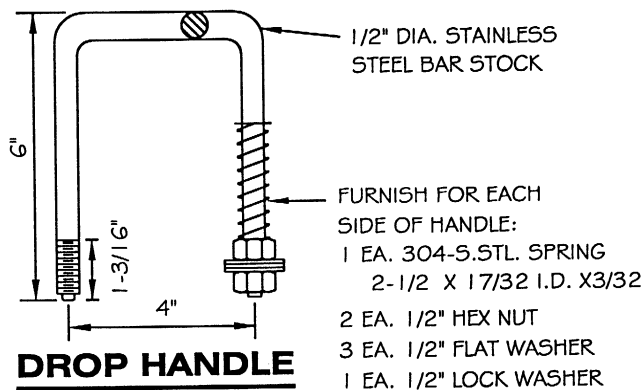


ISOMETRIC VIEW

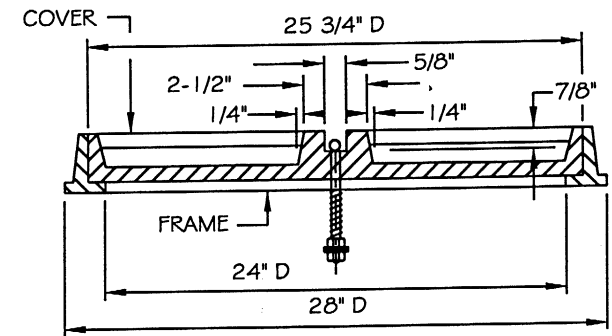
FIBER OPTIC CABLE SPLICING VAULT
UTILITY VAULT CO. #444-TA OR
APPROVED EQUIVALENT



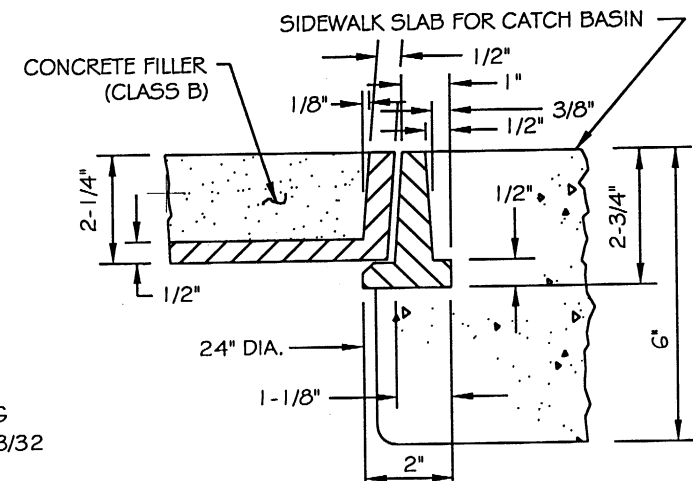
PLAN VIEW



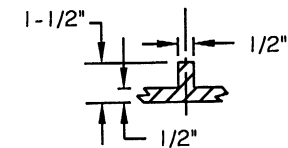
DROP HANDLE



SECTION A-A



SECTION B-B



SECTION C-C

NOTES:

1. FRAME SHALL BE NON-LOCKING.
2. FRAME AND COVER SHALL BE CAST IRON OR ASTM A-36 STEEL. HORIZONTAL SURFACE OF COVER IN CONTACT WITH FRAME SHALL BE MACHINED, ASA B-46 ROUGHNESS SHALL NOT EXCEED 1/32 INCH.
3. COVER SHALL BE FILLED WITH CONCRETE AND BROOM FINISHED.
4. SMALL VARIATIONS IN DIMENSIONS OF FEATURES OF A MINOR NATURE THAT ARE PART OF THE FOUNDRY'S CASTING ARE PERMISSIBLE.

DRAWING COURTESY OF UTILITY VAULT CO.

DETAIL NO.
C-103
NTS

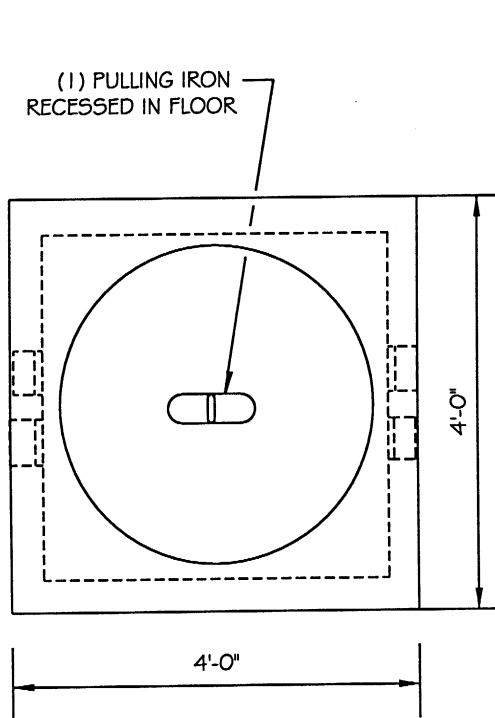


CITY OF
CHANDLER
STANDARD
DETAIL

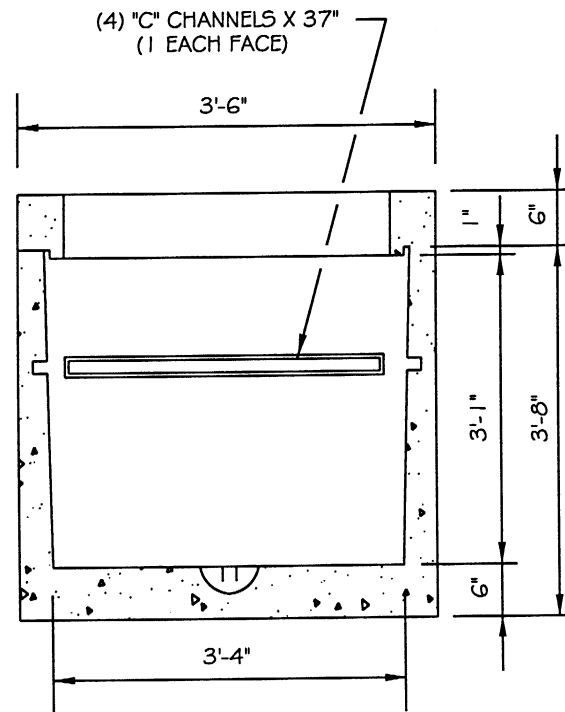
FIBER OPTIC CABLE SPLICING VAULT

APPROVED: *Bryan D. Patterson*
CITY ENGINEER
DATE: 11-19-99

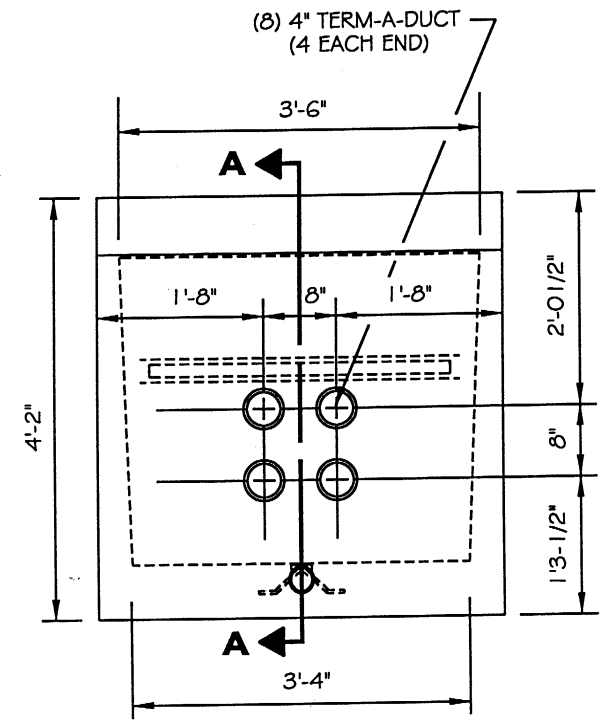
DETAIL NO.
C-103
PAGE 1 OF 2



PLAN VIEW



SECTION VIEW A-A



END VIEW

FIBER OPTIC CABLE SPLICING VAULT
UTILITY VAULT CO. #444-TA OR APPROVED EQUIVALENT

DRAWING COURTESY OF UTILITY VAULT CO.

DETAIL NO.
C-103
NTS

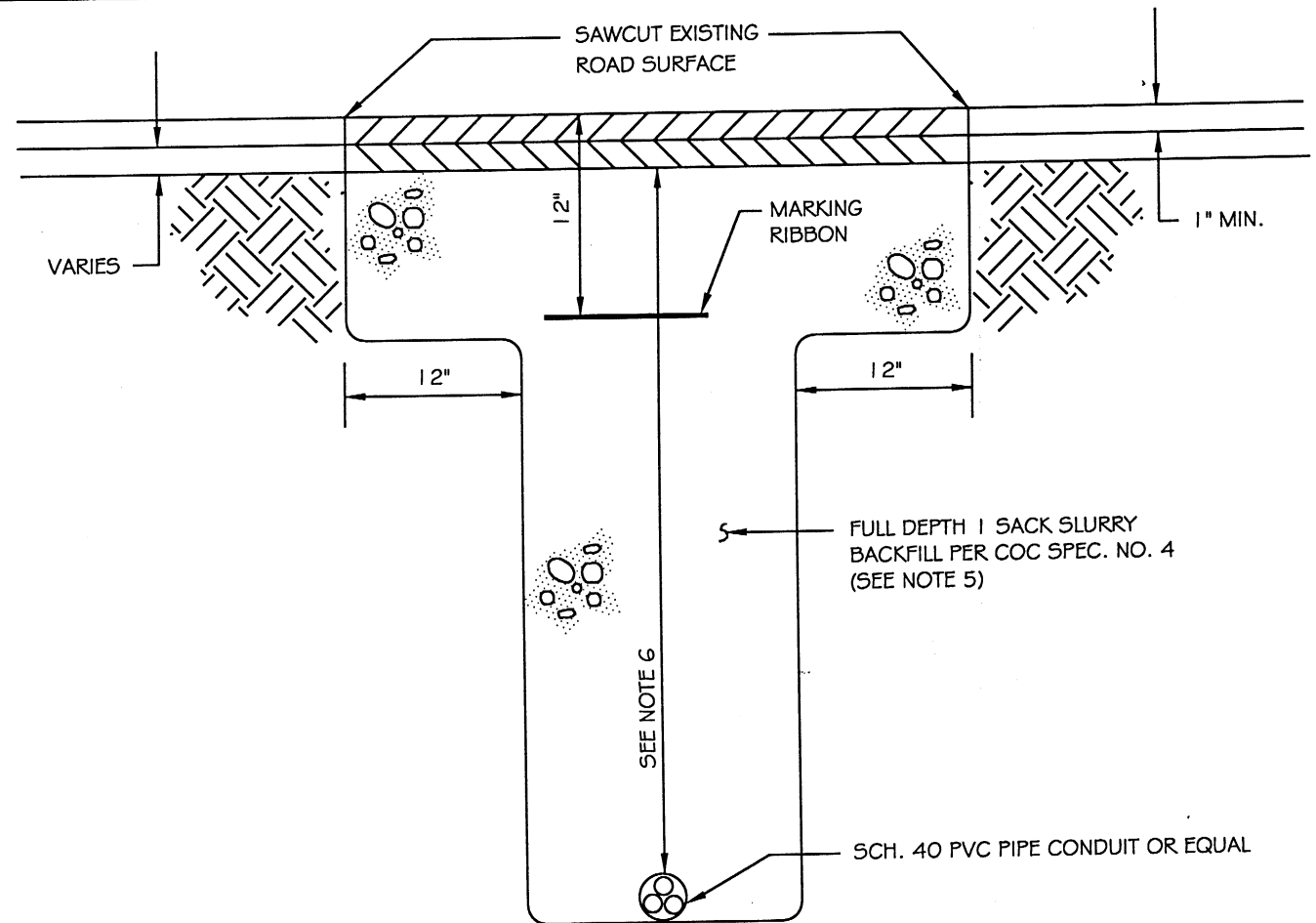


CITY OF
CHANDLER
STANDARD
DETAIL

FIBER OPTIC CABLE SPLICING VAULT

APPROVED: *Bryan D. Holloman*
CITY ENGINEER
DATE: *11-19-99*

DETAIL NO.
C-103
PAGE 2 OF 2



NOTES:

1. MARKING RIBBON TO BE 3" MINIMUM WIDTH, 5 MIL THICK METALLIC DETECTABLE TAPE WITH THE MESSAGE "CAUTION - FIBER OPTIC CABLE BURIED BELOW".
2. FOR BORES OR PUSHES, STEEL SLEEVES MUST BE USED.
3. REPLACE ROAD SURFACE PER MAG STANDARD DETAIL 200 'T' TOP.
4. INSTALLATION SHALL BE PER COC STD. SPEC. #8.
5. NATIVE MATERIAL CAN BE UTILIZED FOR BACKFILL IN P.U.E. OR MIN. 2 FEET FROM SURFACE IMPROVEMENTS.
6. DEPTH SHALL BE IN ACCORDANCE WITH MAG SPECIFICATION 360.

C-104
REPLACES
120



CITY OF
CHANDLER
STANDARD
DETAIL

FIBER OPTIC CABLE DUCTS

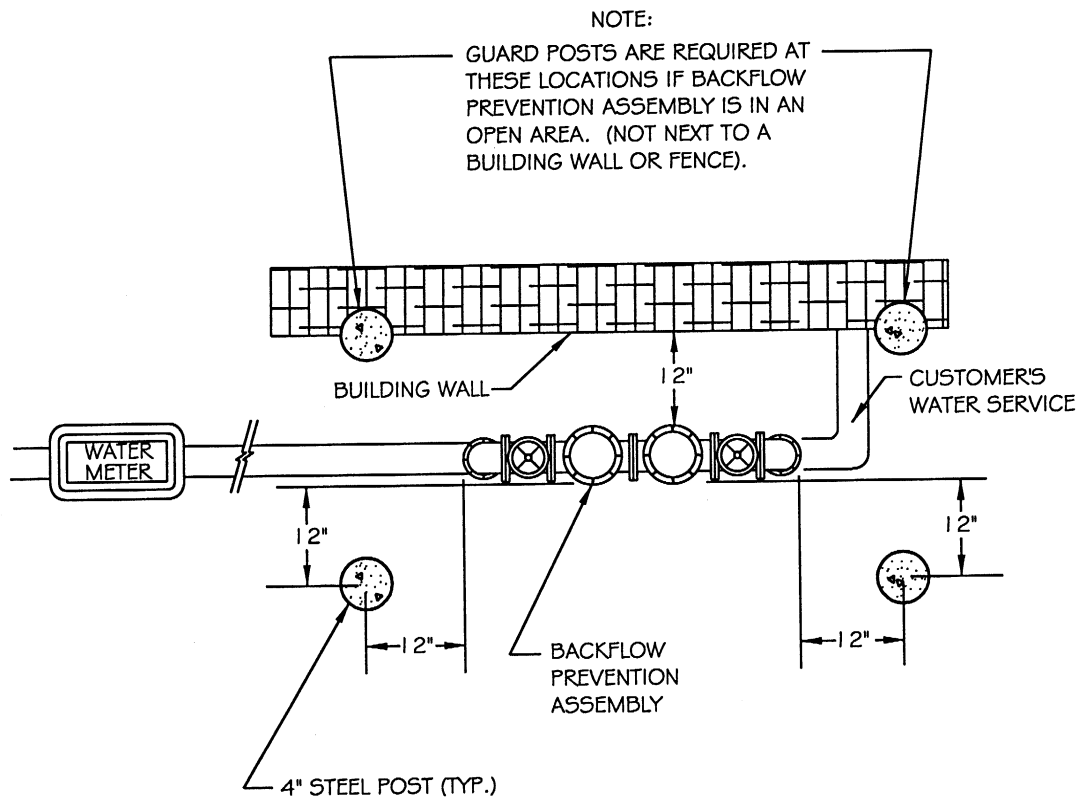
APPROVED:

Dwight D. Patterson
CITY ENGINEER

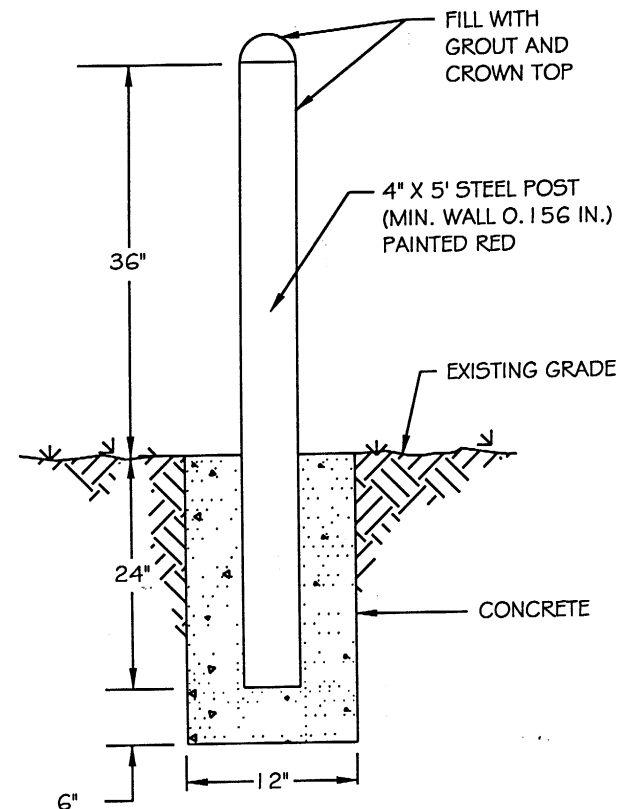
DATE: 11-19-99

DETAIL NO.

C-104
NTS



**GUARD POST FOR BACKFLOW
PREVENTION ASSEMBLY**



**GUARD POST SECTION
(BOLLARD)**

**C-105
REPLACES
94**



CITY OF
CHANDLER
STANDARD
DETAIL

**GUARD POST FOR BACKFLOW
PREVENTION ASSEMBLIES**

APPROVED:

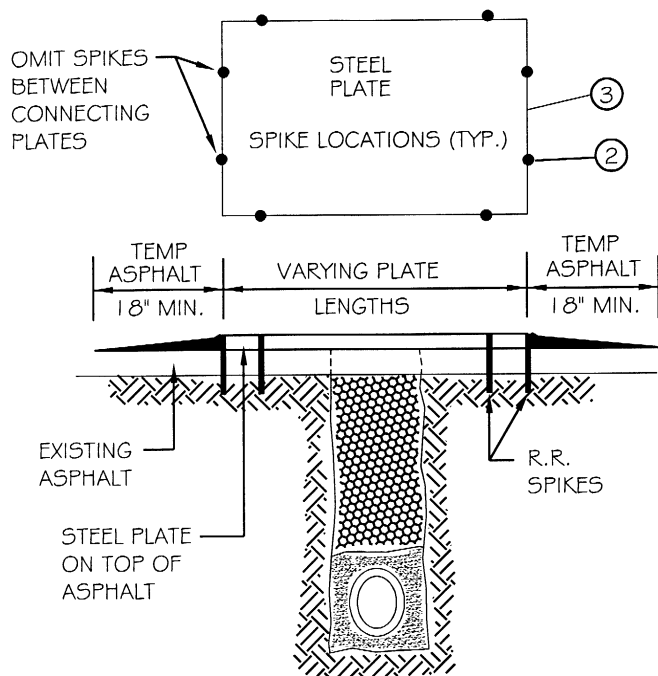
Don D. Peterson
CITY ENGINEER

DATE: 11-19-99

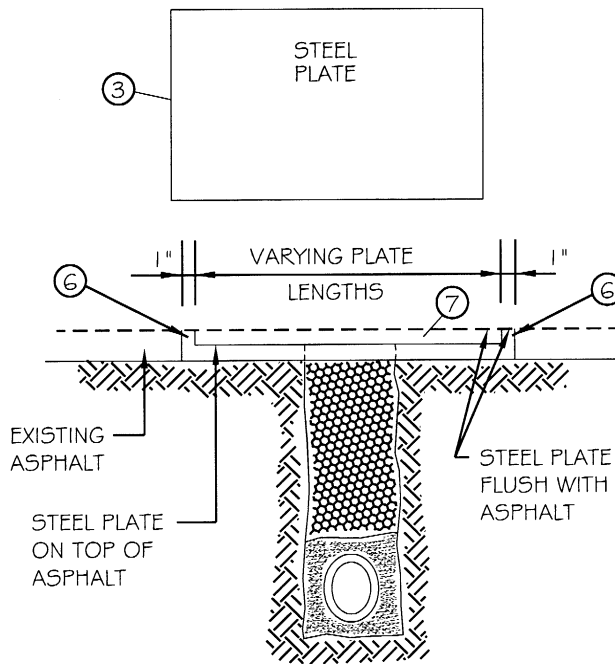
DETAIL NO.

C-105

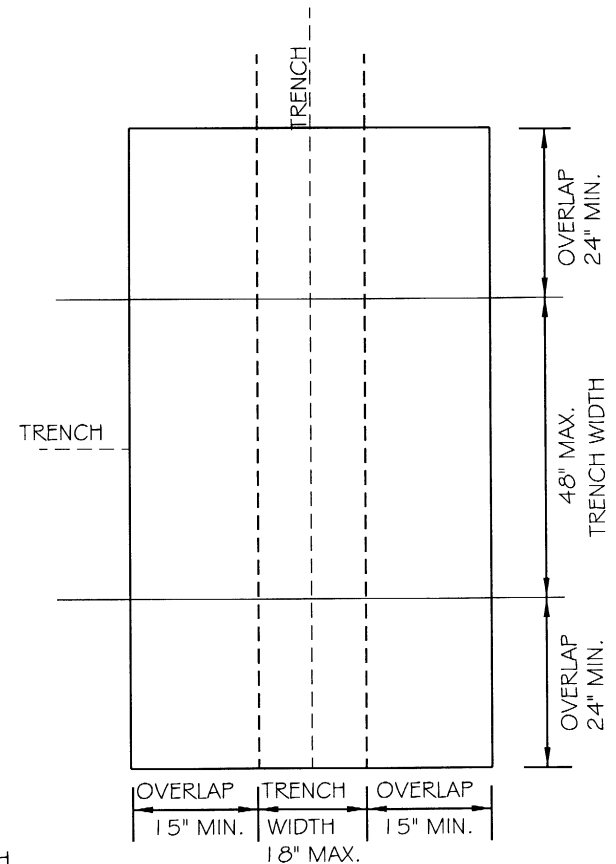
NTS



TYPE 'A'



TYPE 'B'
(SEE NOTE NO. 4)



○ NOTES:

1. USE OF STEEL PLATES WILL NOT EXCEED TEN (10) CALENDAR DAYS, PRIOR TO FINAL PATCH.
2. USE R.R. SPIKES OR SIMILAR FASTENER.
3. TYPICAL PLATE DIMENSIONS 4' X 8' X 1" THICK.
4. USE TYPE 'B' ON STREETS WHERE THE POSTED SPEED LIMIT IS 30 M.P.H. OR GREATER.
STEEL PLATE SHALL BE SET FLUSH WITH EXISTING ASPHALT.
5. TRENCH BACKFILL AND PAVEMENT REPLACEMENT PER MAG STANDARD DETAIL 200.
6. GAP 1" TO A MAXIMUM OF 4", MUST BE FILLED WITH TEMPORARY ASPHALT.
7. STEEL PLATE MUST BE STABLE TO STAND HEAVY TRAFFIC, NOT ALLOWING ANY MOVEMENT.
8. NO MORE THAN 500' OF TRENCH PLATING IS PERMITTED AT ONE TIME.
9. STEEL PLATES CANNOT BE USED FOR SIDEWALKS.
10. RECESSED PLATES REQUIRED FOR BIKE LANES.

DETAIL NO.

C-106

NTS



CITY OF
CHANDLER
STANDARD
DETAIL

STANDARD TRENCH PLATING

APPROVED:

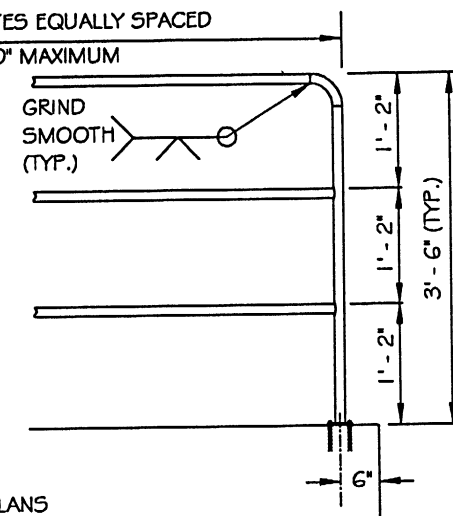
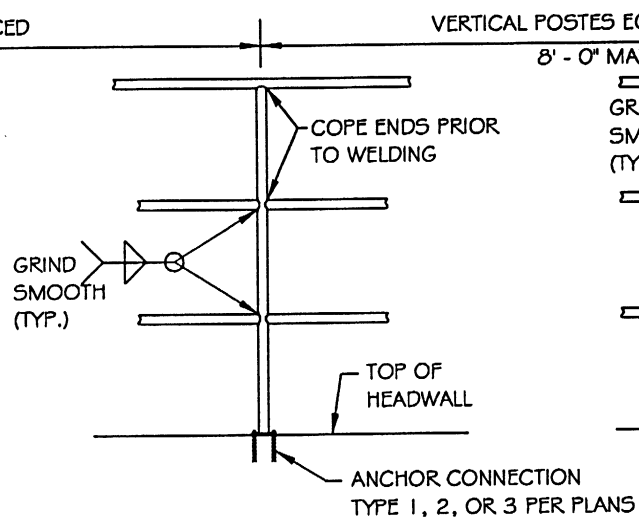
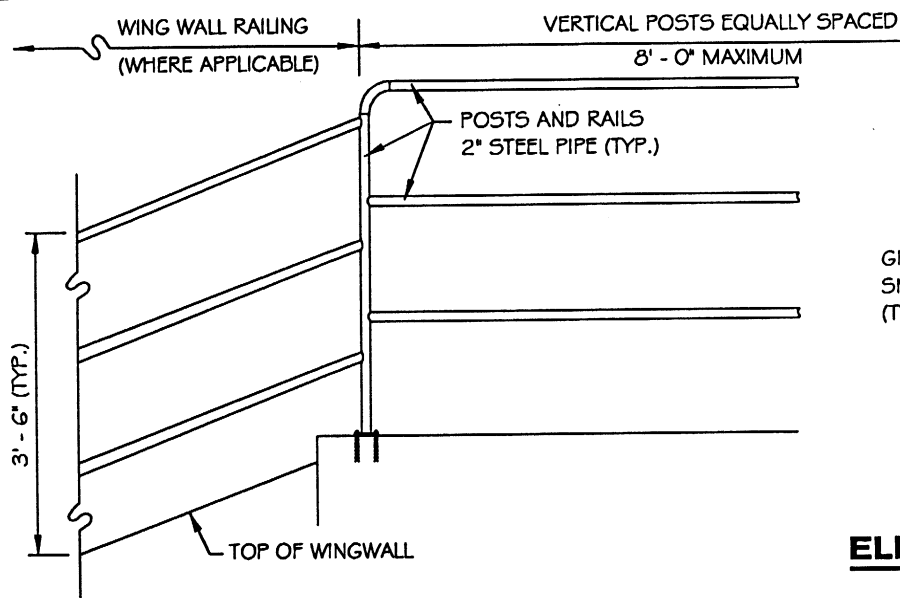
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CITY ENGINEER

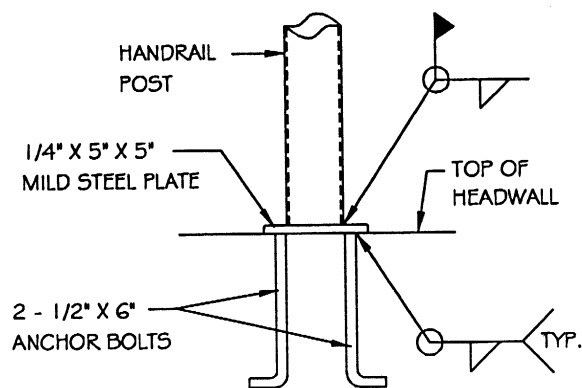
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C-106

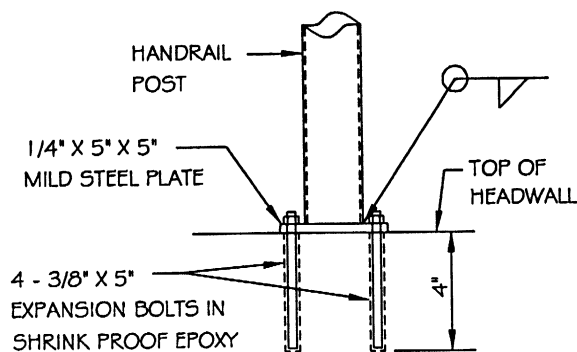
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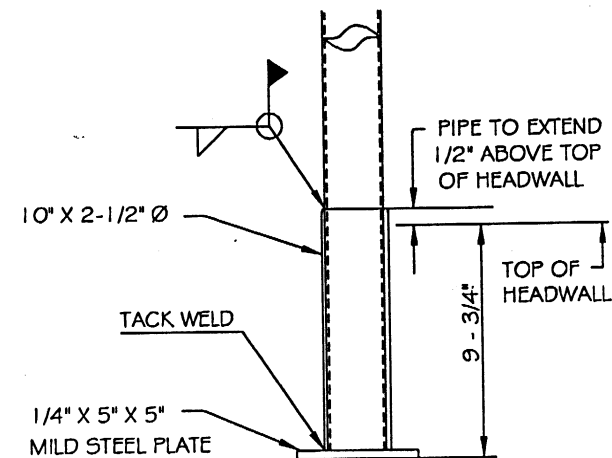
ELEVATION



TYPE 1
ANCHOR PLATE DETAIL



TYPE 2
EXPANSION BOLT DETAIL



TYPE 3
PIPE SLEEVE DETAIL

NOTES:

1. PAINT HANDRAIL PER ADOT SPECIFICATIONS. COLOR PER PLANS.
2. VERTICAL POSTS TO BE EVENLY SPACED.

DETAIL NO.
C-107
NTS

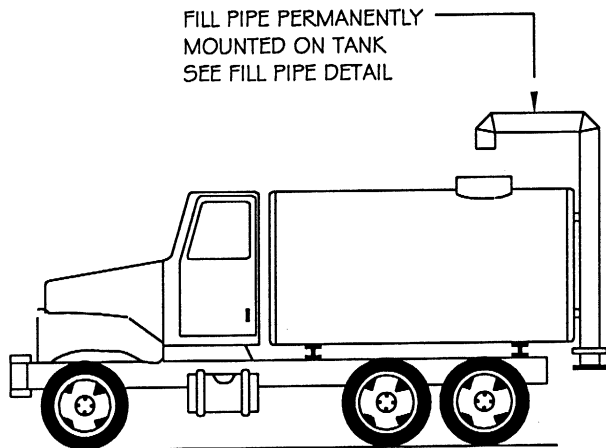


CITY OF
CHANDLER
STANDARD
DETAIL

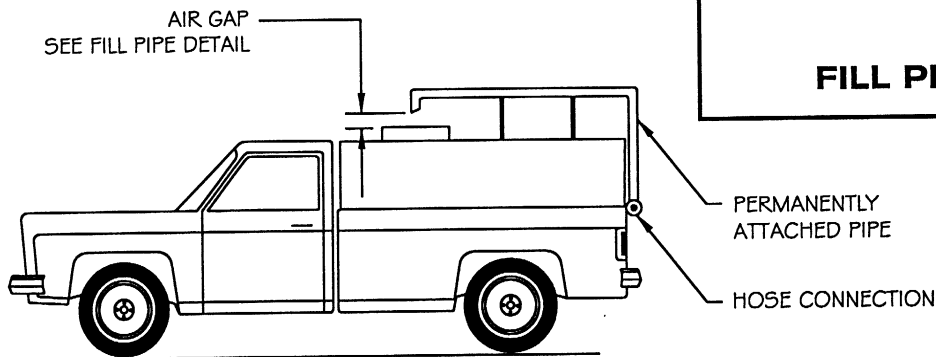
GALVANIZED HANDRAIL

APPROVED: *Bryce D. Paltrow*
CITY ENGINEER
DATE: 11-19-99

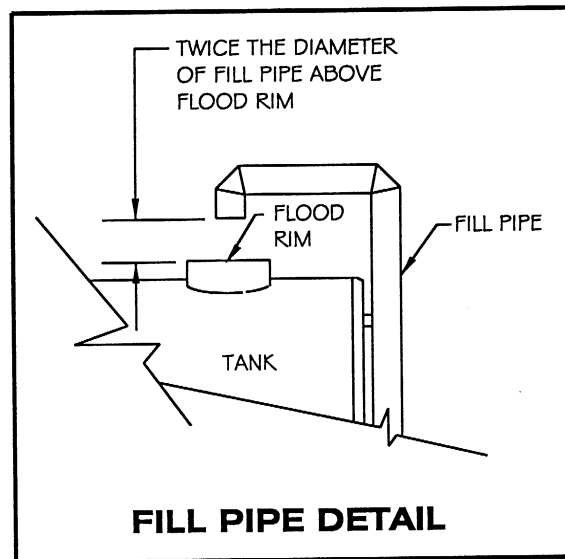
DETAIL NO.
C-107
NTS



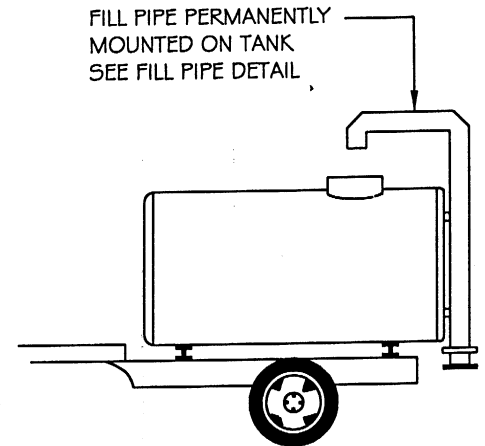
WATER TRUCK



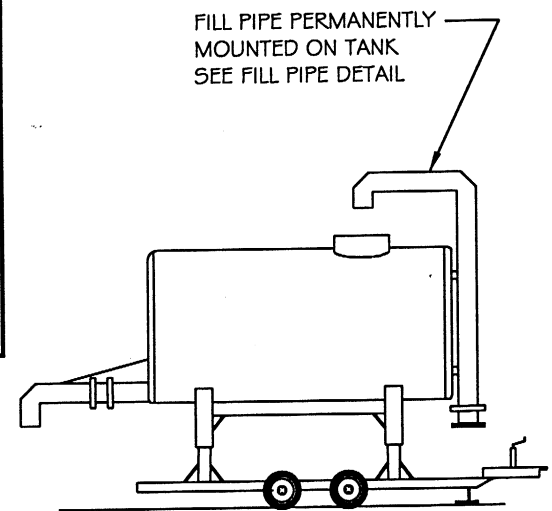
WATER TRUCK



FILL PIPE DETAIL



WATER WAGON



ELEVATED TANK

C-108
REPLACES
95



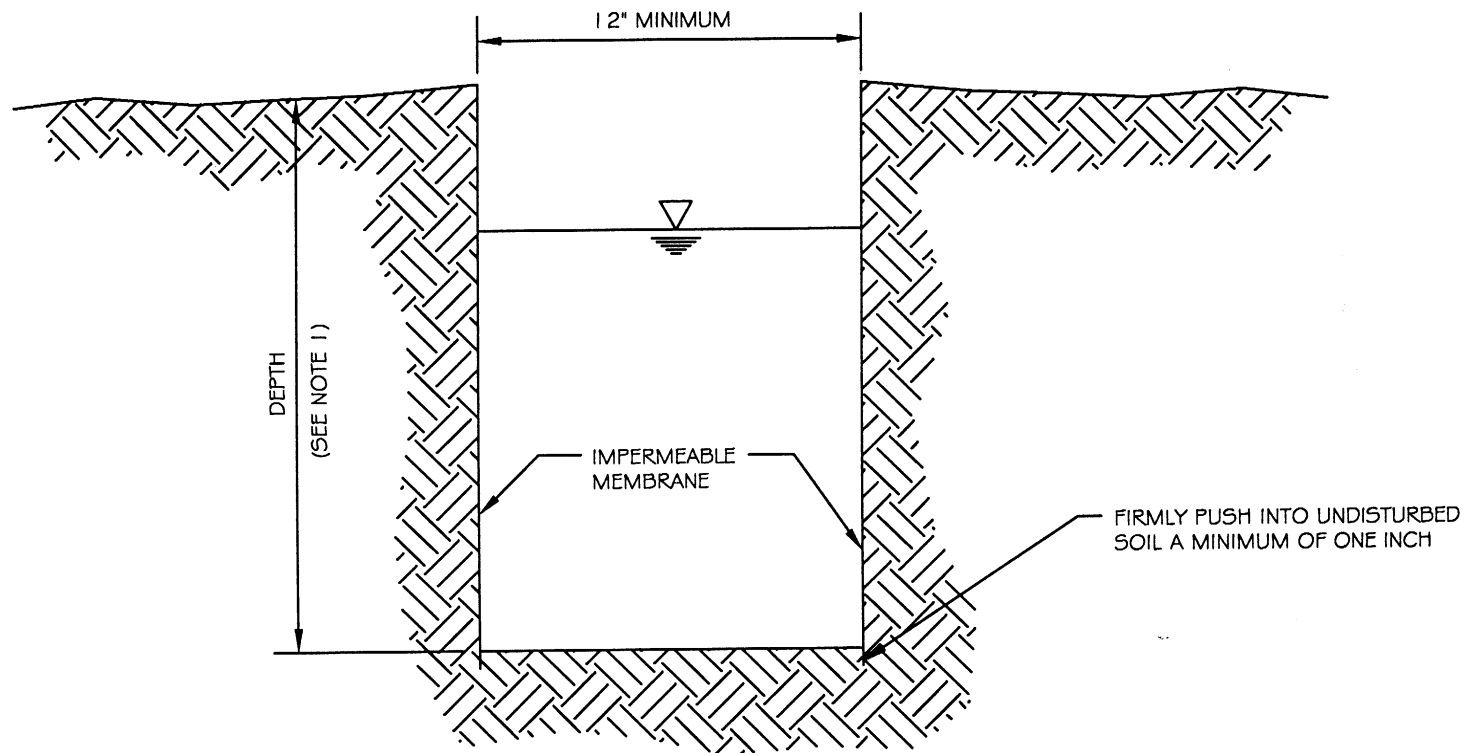
CITY OF
CHANDLER
STANDARD
DETAIL

**AIR GAP BACKFLOW PROTECTION
FOR WATER TANKS**

APPROVED: *Ray D. Peterson*
CITY ENGINEER
DATE: *11-19-99*



DETAIL NO.

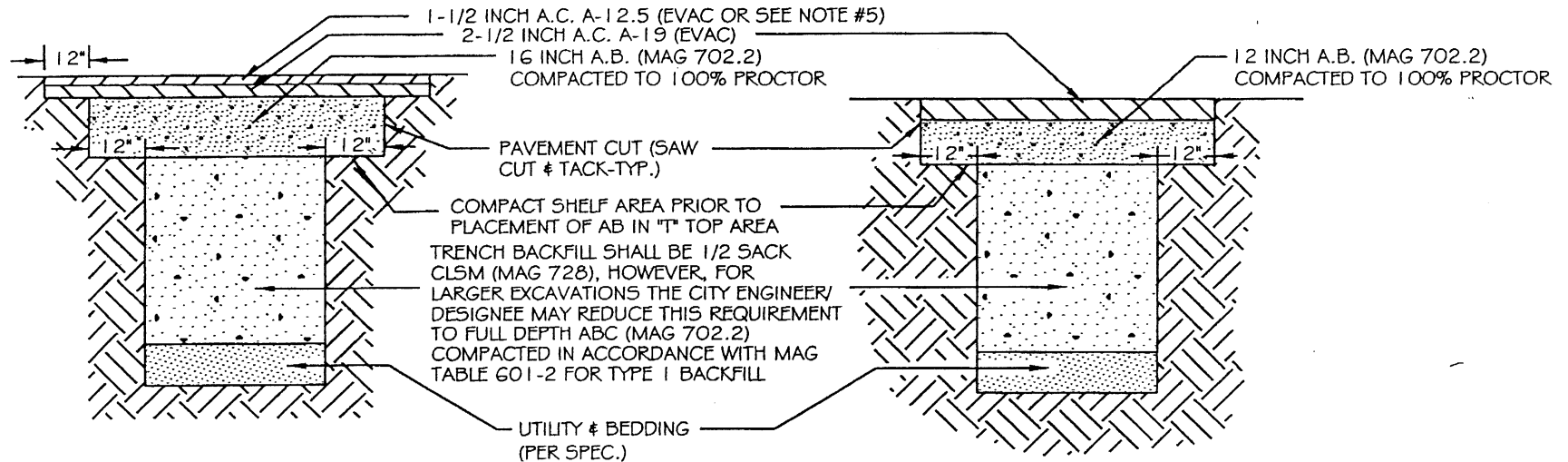
C-108
NTS



NOTES:

1. THE TEST SHALL BE PERFORMED THREE FEET BELOW NATURAL GROUND OR AT THE ELEVATION OF THE BOTTOM OF THE RETENTION BASIN, WHICHEVER IS LOWER.
2. THE TEST HOLE SHALL BE PREWETTED FOR 24 HOURS, OR UNTIL A STABILIZED PERCOLATION RATE IS ACHIEVED.
3. THE TEST HOLE SHALL BE REFILLED DURING THE PREWETTING PERIOD AS NECESSARY TO MAINTAIN A FREE WATER SURFACE. IF AT ANY TIME A FREE WATER SURFACE IS NOT MAINTAINED, THE PREWETTING PROCESS SHALL BE RESTARTED.
4. THE TEST RESULTS ARE TO BE EXPRESSED IN THE UNITS OF CUBIC FEET PER HOUR PER SQUARE FOOT OF PERCOLATION AREA.

DETAIL NO. C-109 NTS	 CITY OF CHANDLER STANDARD DETAIL	SHALLOW PIT PERCOLATION TEST REQUIREMENTS	APPROVED:  CITY ENGINEER DATE: <u>11-19-99</u>	DETAIL NO. C-109 NTS
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PAVEMENT CUT AND PATCH SECTION DETAIL

ARTERIALS, COLLECTORS, INDUSTRIAL STREETS

PAVEMENT CUT AND PATCH SECTION DETAIL

ALL OTHER STREETS

NOTES:

1. ALL TRENCH BACKFILL AND PAVEMENT REPLACEMENT ON EXISTING STREETS SHALL BE AS SHOWN IN THIS DETAIL.
2. PAVEMENT REMOVAL AND REPLACEMENT DETAIL SHALL BE USED ON ALL EDGES INCLUDING TRENCH ENDS EXCEPT WHERE THE EDGE IS PORTLAND CEMENT CONCRETE.
3. WHERE CUT IS 24 INCHES OR LESS FROM THE EDGE OF AN EXISTING PATCH, CURB AND GUTTER, CONCRETE PAVEMENT, CONCRETE CROSSWALKS AND/OR DECORATIVE PAVERS, REMOVE THE EXISTING AC BETWEEN THE CUT AND THIS EDGE AND REPLACE. THE THICKNESS OF THE REPLACEMENT PAVEMENT IN THIS AREA SHOULD BE EQUAL TO THIS DETAIL OR EXISTING AC THICKNESS, WHICHEVER IS GREATER.
4. PLACE AND COMPACT AB IN LIFTS NOT EXCEEDING 8".
5. WHERE PATCHES ARE LOCATED IN STREETS SURFACED WITH RUBBERIZED ASPHALT, A RUBBERIZED ASPHALT MIX DESIGN SHALL BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL PRIOR TO INSTALLATION.

DETAIL NO.

C-110

NTS



CITY OF
CHANDLER
STANDARD
DETAIL

PAVEMENT CUT AND PATCH

APPROVED

[Signature]
CITY ENGINEER

DATE: 11/20/03

DETAIL NO.

C-110

NTS

